

TRANSPORTATION AND ACCESS GUIDELINES

1. HATCHER PASS ROAD

- a. **Degree of Improvement** The Hatcher Pass Road should be upgraded as outlined in the Hatcher Pass Plan. The road will be paved in this section only if the traffic volume is so high that the only feasible way to maintain the road is by paving it. The intent is to keep the road as a low-speed, scenic roadway.
- b. **Pullouts and Parking Areas** Preliminary identification of parking areas (trail heads) has been made based on recreational uses and scenic qualities. These are located in subunit 1, in section 2 (m. 31, north side of road); sections 4 and 5 (north side); section 5 (north side); and section 6 (south side). See Hatcher Pass Management Plan, Map 12. Pullouts may be needed in addition. ADF&G, DOPOR, and DOT/PF will work cooperatively to designate the appropriate vehicle pullouts at locations determined to be good for wildlife viewing during any future reconstruction of the Hatcher Pass Road. The final decision on the location of any road turnouts or parking areas will be made after considering recreational use patterns, scenic views, cost, safety, and roadway geometrics. Pullout or parking areas should not be located where they create traffic safety problems.
- c. **Design Considerations** All reconstruction and maintenance of the Hatcher Pass Road in sections 2-6, T19N, R2W, S.M. should be done in a manner that enhances or, at minimum, does not detract from the scenic views from the road. The following guidelines should be followed to the extent economically and physically feasible, given that the Hatcher Pass Road is an existing road. To achieve these standards, it may be necessary for DNR to provide an easement to allow sufficient room for cut and fill contouring. In steep portions of the road, safety considerations or low level funding may make following design standards infeasible.
 - (1) **Restoration of Adjacent Areas** After construction, the adjacent terrain should be restored to a natural appearing condition as similar to the original as possible.
 - (2) **Contour Grading Plan** The design of the roadway should follow the contour of the topography to the extent that the standards for recreational roads can be met. The intent is that the road should blend with the landscape.
 - (3) **Slope Molding** Excessive fill should be avoided to the extent possible by careful route location. Slope molding techniques should be incorporated to blend the cuts and fills into the adjacent landform. Examples include the following:

Cuts perpendicular to creeks or draws should be incised upstream to create a natural appearance along the confluence. Short ridges between draws may be elongated and rounded to a convex form according to the surrounding terrain. The appearance from the road should be as if the ridgeline curves and flows smoothly downward and connects with the draws as if naturally, rather than that of harsh vertical lines.

Knobs left on a downhill side of a road cut should be removed to accentuate desirable views or provide turnouts. Exceptions with particular visual interest may be considered.

- (4) **Revegetation/Landscaping** Disturbed areas will be revegetated to provide soil and slope stability, improve the view, reduce maintenance, and provide food and cover for wildlife. See Appendix IV for details.
- (5) **Clearing** Selective vegetative clearing and thinning should be implemented to reflect the vegetative patterns in the vicinity. At locations where the roadway slopes downward or curves to reveal a spectacular view, clearing may be practiced to provide viewing opportunities or to frame the overlook.

Thinning and clearing may also be practiced in communities where aboriginal species predominate, e.g., willow or alder. Selective clearing provides increased sunlight, decreased moisture, and conserves soils that would be lost if the area were clearcut and revegetation expenses eliminated.

- (6) **Rock Cuts** Some slopes may require rock cuts. Blasting techniques should be used that expose natural rock fractures, display geologic history depicted by the existing strata and prevent vertical drill marks on the walls. Where rock cut is required at a draw or spring, the water feature should be accentuated; an example would be creating a waterfall design. Cuts exceeding 20 vertical feet should be created in staggered benches, sculptured similarly to the adjacent topography. The benches should be provided topsoil and seeded. To minimize the size and scale of the rock cuts, trees or shrubs may also be considered on the benches.
- (7) **Erosion Control** Temporary or permanent erosion control practices should be required and should be designed to be pleasing to view. See Appendix IV for details.
- (8) **Bridges** Bridges should be designed to match the character and contour of the topography. They should be consistent with the curvilinear design of the contour grading plan. Minimal ground disturbance should be emphasized. Fill should be used at a minimum to promote a spacious effect and provide anticipated viewing opportunities. Disturbed ground should be recontoured and revegetated similarly to that of the adjacent terrain.

2. TRAILS MANAGEMENT

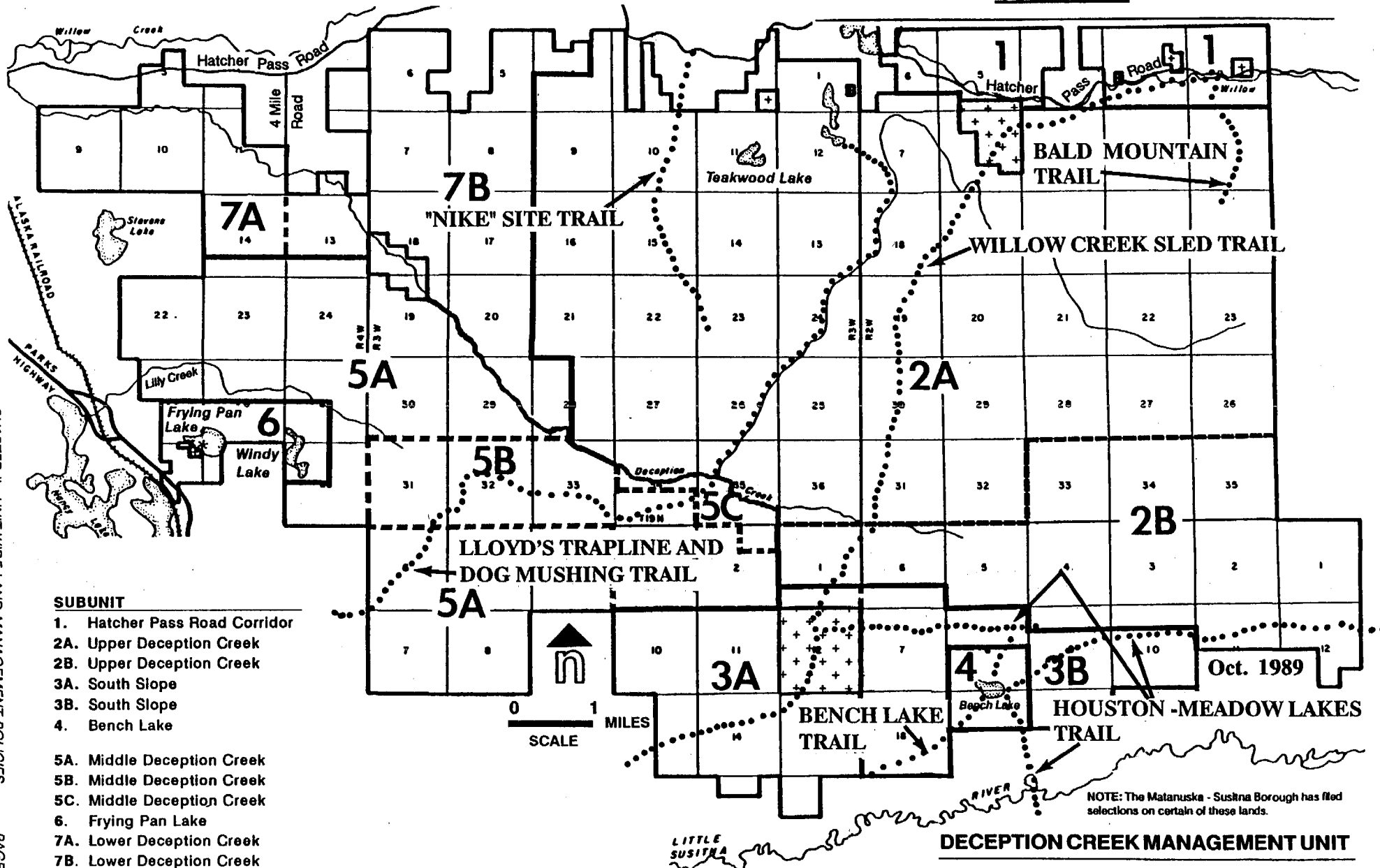
- a. **Parking** Developing public parking facilities is a priority before trails will be upgraded.
- b. **Buffers** Recreational trails recognized by this plan will have a minimum 50 foot vegetated buffer on each side of the trail center line. The intent is to maintain a natural appearing landscape. This guideline should be reviewed at the time this plan is revised to determine whether a wider buffer is needed. That may be the case if it is decided that some of the land is not to be retained in public ownership or if the use of the trails increases. The trails to which this guideline applies:

Willow Creek Sled Trail
"Nike Site" Trail
Bench Lake Trail
Houston-Meadow Lakes Trail
Lloyd's Trapline and Dog Mushing Trail
Bald Mountain Trail

Activities allowed within trail buffers include the following:

- (1) Disease and insect control and prevention with pesticides or selective tree cutting as approved by ADF&G, Division of Parks and Outdoor Recreation, DLWM and DEC and subject to the requirements of interim forestry guideline number 1 f (1)-(4).
- (2) Other recreational trails may cross these trails and their buffers.
- (3) Logging roads and utility lines may cross these trails and their buffers if no feasible and prudent alternative exists. Such crossing should be at 90° or as close to that as possible.
- (4) Material sites solely for the purpose of maintaining the trail.
- (5) Habitat enhancement activities that involve the conversion of brush are allowed.
- (6) Selective tree cutting (single tree selection cuts) provided that it does not substantially alter the appearance of the woods from the perspective of the trail user.
- (7) Grazing as approved by the Division of Land and Water Management, in consultation with ADF&G and the Division of Parks and Outdoor Recreation, and consistent with the grazing guidelines of this chapter.
- (8) Recreational facilities associated with the trail. Exceptions may occur in special recreation use areas.

TRAILS



SUBUNIT

1. Hatcher Pass Road Corridor
- 2A. Upper Deception Creek
- 2B. Upper Deception Creek
- 3A. South Slope
- 3B. South Slope
4. Bench Lake
- 5A. Middle Deception Creek
- 5B. Middle Deception Creek
- 5C. Middle Deception Creek
6. Frying Pan Lake
- 7A. Lower Deception Creek
- 7B. Lower Deception Creek



Private, university, or borough land



2.3 acre parcel of state land on Frying Pan Lake

- (9) Trail maintenance.
 - c. **Implementation of restrictions** Implementation of the plan's restrictions to the trails listed in guideline number 2b above will be through the local platting authority.
 - d. **Maintenance** Trails will be maintained as funds and volunteer support permit.
 - e. **Volunteers** Agreements with volunteer groups for trailwork will specify that the group has no exclusive rights to use.
 - f. **Requirements** All recreation trails shall:
 - (1) Be open for public use.
 - (2) Have established legal access along the route through private and public lands.
 - (3) Provide parking at designated trailheads.
 - g. **Uses** The trails listed above may be used for access for logging subject to the following guidelines:
 - (1) Loggers should use only one side of the trail where it is wide enough and leave the other side of the trail for recreational use, particularly dog mushing.
 - (2) Loggers should avoid constructing berms that would block the use of the trail by mushers or other recreational users.
 - (3) The Department of Natural Resources should avoid authorizing the use of trails used by mushers if the trail is not wide enough to accommodate both uses.
3. **RESOURCE ACCESS ROADS** Because of the high wildlife values of these subunits, the following guidelines apply to two types of new roads: year-round roads and winter or temporary roads. If a road is built for year-round travel it is almost impossible to close it down and make it a temporary road in this management unit because there is no major physical barrier such as a river. Therefore, unless DNR decides that a year round road is either desirable or not objectionable, roads built to access resources will be winter roads.
- a. **Subunits 2A, 2B, and 5C** There will be no year-round roads in subunits 2A, 2B and 5C unless necessary for the development of a major mineral discovery. Temporary winter roads in these subunits will be designed and located so as to have minimal impact on moose and important moose winter habitat. In order to ensure that this standard is met, ADF&G will review the design and location of any temporary winter road in these subunits and must agree that the impact on moose and moose winter habitat is minimal. This policy applies to subunit 5C so long as it is in state ownership. It is recognized that subunit 5C is a borough interest area; it is not intended that this policy apply if the subunit becomes borough land.

- b. **Minimum Amount Necessary; Winter Roads Preferred** Access roads for resource development will be kept to the minimum amount necessary for resource development. Road construction just for mineral exploration will not be allowed. Winter access roads will be used whenever possible unless DLWM determines that a permanent road is desirable. In making that determination, DLWM will consult with ADF&G and DOPOR and will consider the following criteria listed below; it is not intended that all three criteria must be met in every case:
- (1) the road provides desired recreational access or facilitates appropriate levels of harvest of wildlife;
 - (2) an evaluation of wildlife needs determines that the road will have a positive effect on wildlife or will not have a significant adverse effect; the evaluation will be done by ADF&G;
 - (3) whether maintenance will be needed and the availability of funds for maintenance.
- c. **Design and Location of Roads** Roads will be designed and located in a manner that minimizes soil erosion, visual impacts, and impacts on hunting, trapping, fish and wildlife, and their habitat. Where feasible and desirable, a road will be located so that it provides access for more than one purpose. For example, a well-placed road could serve the miner and also provide motorized access for grazing, hunting, and other recreational activities. An analysis of proposed roads in areas where wildlife habitat or public recreation is designated as a primary use will be performed by ADF&G or DOPOR, as appropriate. The analysis will include recommendations for routing and design of the road in a manner that will enhance or have minimum impact on wildlife, fisheries, and recreation values. Where desirable and feasible, access roads will be located so as to provide destination or loop recreation trails.
- d. **Public Access** Resource access roads will serve as public access except when
- (1) a determination is made by DNR in consultation with ADF&G that public use would negatively impact wildlife populations or recreation; or
 - (2) necessary to protect public safety; or
 - (3) necessary to protect the integrity of the roadbed.
- e. **Future Closure** When access roads are detrimental to management of wildlife or public recreation, DNR may require that they be rehabilitated upon completion of resource development.

- f. **Pre-Construction Approval** Proposed roads will be located on a topographical map and submitted for interagency review as part of the review of the plan of operations, miscellaneous land use permit, right-of-way permit, timber sale, or other proposed action.
- g. **Stream Crossings** Culverts and/or bridges will be used at all stream crossings of year-round roads and shall follow ADF&G standards for fish passage. Winter roads that cross anadromous fish streams require Title 16 permits; on these, crossings alternatives to culverts and bridges may be used if acceptable to ADF&G.
- h. **Low Speed Design** Access roads will be designed to constrain high speeds by motorized vehicles.